

Public Consultation Statement

Development Proposal

Ellis Farm, Kilby Bridge



1.0 INTRODUCTION

This Statement has come from the new local plan public consultation documentation issued for formal representations by stakeholders. This document refers to the Kilby Bridge sections of the Local Plan – Pre-submission Consultation Document received from the council in November 2017.

The purpose of this document is to provide relevant evidence and justification for the site and the design process undertaken and the principles that have led to this development proposal.

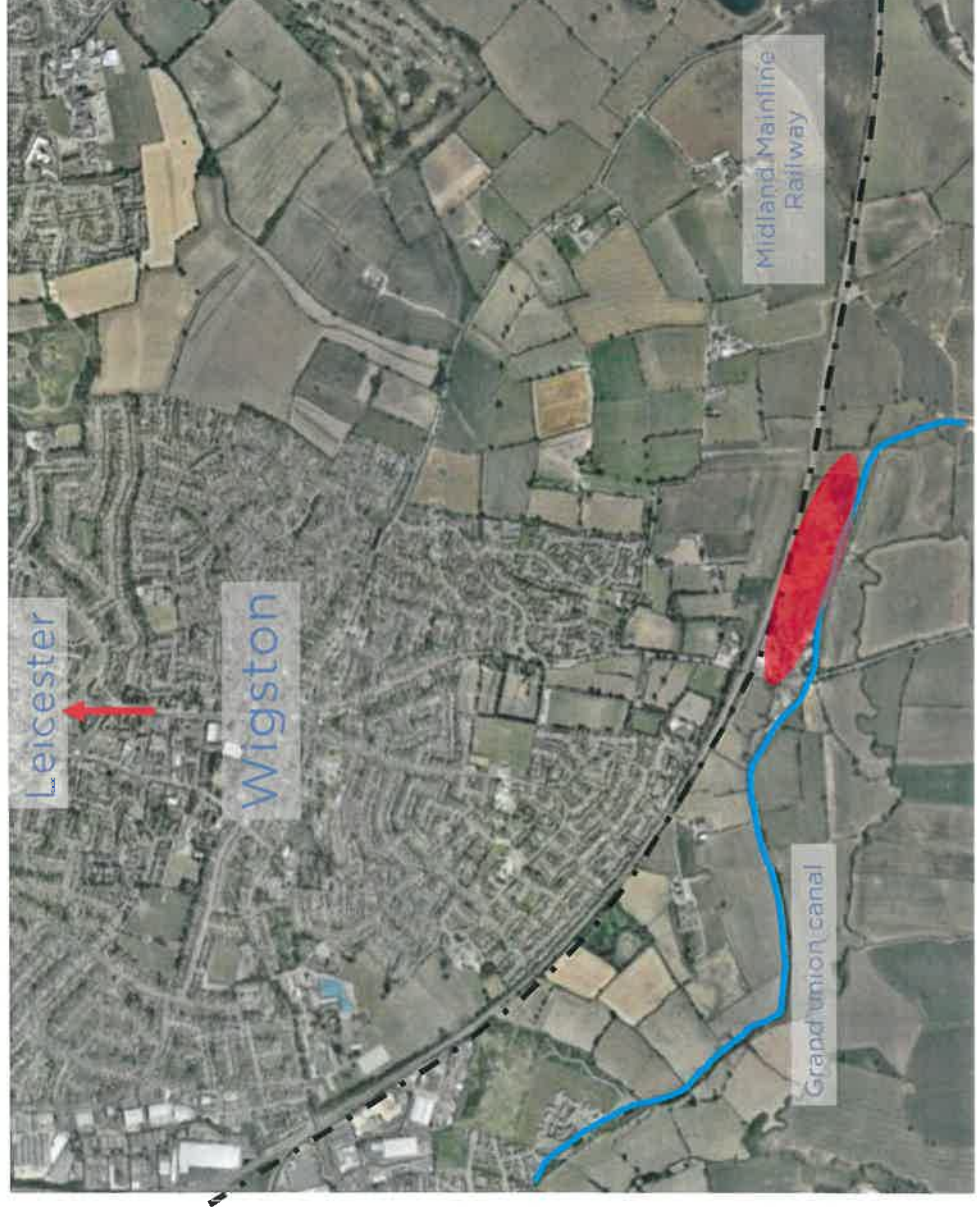
GWH and clients have been in consultation with the local planning authority and the policy team for a number of years specifically in relation to the possible extension of the Kilby Bridge village envelope to provide additional housing within the Oadby and Wigston area which is underprovided for at the current time. The new local plan was issued to address this shortfall in provision but all submission made regarding our site have been ignored and the local plan has gone further in changing the designation of green belt land into a conservation area. This conservation area is currently a horse field, menage and stables.

This document goes some way in clarifying the area with regards to the possibility of changing the designation and extending the current boundary of the Kilby Bridge settlement to include some of the Ellis Farm area into a new settlement boundary with low level housing and conversions with the possibility of opening up some badly needed public accessible open parkland.

This document is to be read in conjunction with drawings referenced 6193 – 500, 501, 502 and 503.

2.0 ASSESSMENT

2.1 Assessment – Site Location



The site is located to the south of Leicester, in the suburb of Wigston at Kilby Bridge.

Wigston is a town within its own right having a centre with food and high street retail shops and other services including schools, dentists and medical centres. Kilby Bridge also has a public house.

The site forms part of land owned by the applicant in this location. Indeed it is partially bound to the west by dwellings in the applicant's ownership that front the A5199 Welford Road. The site is bound to the south by the Grand Union Canal and to the north by the midland mainline railway.

2.1 Assessment – Site Description & Local Character

Site Description

The site currently consists of; stables and associated courtyards and paddocks, grass pastureland, and a Site of Specific Scientific Interest comprising a small nature reserve and pond.

The stables have previously been associated with a farm house fronting onto the Welford road.

The open pasture land, with trees to its boundaries, continues to the east of the site with the small nature reserve and pond beyond.

The site and land to the north and east forms part of the Grand Union Canal conservation area. The Grand Union Canal forms the southern boundary to the application site.

A car dealership, with associated sales buildings and external hard standing for vehicle storage, is located adjacent the railway line

To the west of the site is the hamlet of Kilby Bridge. This is made up of a small collection of houses, a public house - which is the oldest building in the settlement - and the car dealership. There is also a small privately owned boat yard and house to the west of Welford Road.





Grand Union Canal (SSSI) – view north towards site from public towpath



Lime Delves (SSSI) - Small nature reserve & pond within the site



Stables – view towards Plot 4₁ from north



Stables – view towards Plots 2 and 3 from north



Paddocks – view west across paddocks towards rear of dwellings on Welford Road



Open pasture land – view towards site with rear of dwellings to east of Welford Road visible beyond

Local Character

Kilby Bridge is a small settlement with a mix of businesses and residential housing. It is a linear settlement along the Welford road with the eastern boundary being defined by the car dealership, a terrace of four post-war two-storey houses, a two-storey detached farm house (Ellis Farm), and semi-detached 1.5-storey houses.

To the west; The Navigation public house and 2no. two-storey dwelling houses form the extent of buildings fronting Welford Road. Between these buildings and the railway bridge to the north there is a mature hedgerow with gardens associated with 2no. houses set-back from the road.

The southern boundary is defined by the Grand Union Canal and the bridge that crosses it and the northern and eastern extents by paddocks and pastureland.

The small road frontages of the hamlet have adequately maintained buildings; however, many are of low architectural quality, unsympathetic extensions and modernisations have resulted in the loss of character to the properties and thus the settlement.

The large farm house of Ellis Farm and Berry House opposite are now empty and have been boarded up, further reducing the quality of the street scene. Ellis farm house has been extended beyond any recognition of its original form.



Car dealership – east of Welford Road, application site to south



Post-war terraced houses – east of Welford Road



The Navigation and dwellings – west of Welford Road



Berry House – west of Welford Road



1.5 storey cottages – east of Welford Road

The 1.5 storey cottages adjacent to the canal have received new windows and a roof which has taken any real character from them although they do still inform the scale of the area. The terrace to the east is typical post-war local authority housing, which although well maintained is not of a high architectural quality. None of the buildings in the settlement are listed.



Kilby Bridge – crossing the Grand Union Canal

The canal can be viewed from the bridge to the East and also from the bridge and across the car park from the pub to the west. However, there is little other accessible frontage or views of the canal due to it running at 90° to the settlement and also the overgrown nature of the surrounding trees and shrubs.

There is opportunity to walk down to the canal from Kilby Bridge via access points to the tow path from the bridge however there is limited parking other than the private car park to the public house. This access would also be too far to walk to from Wigston which suggests use is limited or an illegal parking solution is utilised.

2.1 Assessment – Site Topography

With the exception of an embankment to the canal the site area is predominantly level. Land beyond the north and west site boundaries generally slopes up away from the site to the north at a gradient of approximately 1:150.

Along the northern site boundary the site slopes up from the access off Welford Road to approximately 1/3 of the way across the site where it levels off. This level continues towards the eastern boundary up to the Lime Delves pond, at which point the levels fall by approximately 3metres to the eastern boundary.

There is a slope down from north to south at a gradient that begins at approximately 1:150 to the west of the site, and increases to 1:50 towards the eastern boundary. An embankment down to the canal begins approximately 6metres from the southern site boundary and transcends around 3metres at the rear of the stables. However, the size and level difference of the embankment reduces towards the east of the site.

2.1 Assessment – Planning Policy

Oadby and Wigston borough council have adopted their core strategy as of September 2010. It refers to Kilby Bridge as having a special and unique character as it is the borough’s only rural settlement.

The extract from the core strategy adjacent shows the location of the site in the planning context of the borough.

The core strategy notes “Kilby Bridge is a unique settlement in the Borough and due to its character and setting; it is an attractive and valuable asset.” and also states;

There is an opportunity to improve and revitalise the character of Kilby Bridge which is the only settlement in the Borough outside of the Leicester Principal Urban Area;

A need to protect Kilby Bridge’s environmental and biodiversity qualities;

Site Analysis



An opportunity for Kilby Bridge to play a greater role in terms of tourism and providing access to the countryside and the Grand Union Canal. Despite being an important location, the core strategy does conclude, as we have in our assessment of the settlement that;

“It (Kilby Bridge) contains a number of uses which do not contribute positively towards the character of the settlement. Therefore, it is important that only appropriate development, such as small scale leisure, tourism and canal based development occurs within the settlement in the future. A Regeneration Scheme should be prepared for Kilby Bridge and its environs covering these issues, plus Strategic Green Infrastructure issues including The Grand Union Canal and Limeselves Site of Special Scientific Interest, Barn Pool Meadow Local Wildlife Site and its associated Regionally Important Geological Site and linkages between the Direction for Growth and Kilby Bridge. More detail regarding a Kilby Bridge Regeneration Scheme and its implementation will be provided in the forthcoming Allocations Development Plan Document.” This has been enhanced in the emerging local plan.

The core strategy also states that;

“The Spatial Strategy aims to develop links between areas of Green Infrastructure into Wigston, Oadby and South Wigston and also into Leicester City. Similarly, proposals for Kilby Bridge should retain or enhance linkages to the Grand Union Canal and River Sence, and take into account the River Soar and Grand Union Canal Strategy.”

Overall the local authority view the area of Kilby bridge as an important location where there is opportunity to develop and improve links to the open countryside, the canal and open space beyond.

The core strategy understands that for the potential of Kilby Bridge as a tourist destination and also to maximise it as a rural link for the urban settlements to the north, some careful development will need to be allowed and that development needs to be closely monitored and parameters for development identified early on;

“Currently, the area around Kilby Bridge consists of scattered residential, commercial and light industrial development, most of which are associated with the canal. With the area likely to become under increasing pressure, consideration will be given to establishing a settlement boundary in the Allocations Development Plan Document. This will safeguard the area from detrimental expansion into the surrounding countryside and loss of valuable green space.”

“In order to provide a planned approach for the Kilby Bridge area, a regeneration scheme will be developed for the settlement and the adjoining area incorporating the Grand Union Canal. This approach will enable consistent growth of the area that meets community needs and ensures the individual character is not diminished. In addition, the potential to incorporate development related to tourism and the canal would revitalise this southern area of Borough and provide employment and leisure opportunities for local people”.

These strategies and the plan above gives a clear indication that the borough intends to encourage growth to the south towards Kilby Bridge and that Kilby Bridge is an important settlement, in its own right, which the local authority also wish to grow.

It is also clear that any development would need to enhance the settlement and improve access to the countryside including the canal and Lime Delves Site of Special Scientific Interest. Refer to the later sections of this report which highlights some shortfalls in the emerging local plan.

Oadby & Wigston Concept Masterplan.

The Councils concept masterplan (See Left) looks at how the land between Newton Lane and Welford Road could be developed. The masterplan shows new housing areas along with new employment land to the south of the site off Welford Road.

New recreation land with a community hall and allotments would be provided and there is the opportunity for a new local centre off Cooks Lane including a primary school, local shopping and community facilities to serve both existing and future residents in the area.

Development of the area would allow for a new link between Newton Lane and Welford Road through the development.

The site proposed is well located for inclusion within this master concept plan utilising an isolated area of land between the railway line and the Grand Union Canal. The proposals are sympathetic to the ecological and rural nature of the site whilst addressing the known housing need.

The addition of Kilby Bridge into the new local plan has highlighted a number of issues that would become apparent as part of the plan proposals, this is mainly how the Kilby bridge area could be enhanced, developed and evolved into a better gateway into Wigston.





Policy 7.2 Kilby Bridge Village Envelope.

The current Kilby Bridge village envelope is indicated in the top diagram shown in the yellow..

The development proposal partly fall within the village envelope but we would highlight a potential enlargement of the village envelope as per the pink area which incorporates an area of land ideally situated for inclusion. The existing use of this land is a set of stables, horse ménage and paddock land. This is currently indicated as being a conservation area within the new proposed local plan. This area is in private ownership and does not allow for any access to these areas for the benefit of the local population.

Any proposals to this area would obviously need to take into account the rural landscape and character of the Grand Union Canal and promote the countryside views to the south of the site.

Any proposals should also seek to sympathise and where possible enhance the ecological status of the site. Our proposed master planning for this area allows for a better utilisation of the site.

2.2 Evaluation

The following factors have led to the evolution of the scheme as it is currently proposed;

Residential Need:

The strategic housing needs of Oadby and Wigston Borough determines that increased housing provision is required to meet demand. Whilst a proportional increase in affordable housing has been identified the site location is not deemed appropriate for a development that would require significant infrastructure in the locality, such as; schools and medical centres, particularly when considered in the context of the neighbouring planning permission (described in section 2.2). Private dwellings would encourage lower densities, enhancing, and remaining in character with, the location.

Sustainability:

In order to address central government demands for sustainable development, outlined in PPS1, this proposal seeks to reuse existing, redundant, buildings by adapting them for residential purposes. The new-build dwellings will seek to exploit the latest energy efficient technologies, whilst utilising site orientation and topography to ensure high levels of self-sufficiency within the development.

Visual impact:

Part of this development proposal will utilise existing buildings and therefore any visual impact in this respect would be through the general refurbishment and improvement of the material appearance of the buildings.

Whilst low density new-build housing will naturally afford visual permeability across the site, the proposals will seek to further reduce the perceived density of the development by offering housing designs in-keeping with the existing and surrounding landscape and aesthetics. The current local plan requirement for a minimum of 30 per hectare does not afford this limitation of visual impact.

2.3 Design

Site Layout

The proposal will utilise the existing infrastructure beyond the site boundaries. The application site is comprised of private housing plots to the rear of existing buildings to be converted. It would also allow for either small scale individual housing as shown or housing and residential elderly accommodation which is lacking in the area.

Design & Materials

The proposals for the converted stables will seek to retain and enhance the agricultural aesthetic. Enhancements to the external appearance will be limited where possible. However, the fabric does need upgrading in some areas where it has deteriorated. New materials will be selected appropriate to the agricultural / rural context, with straw bale walls and corrugated materials as an example.

The new-build elements, as described in section 2.3, will comprise design features and materials that are sympathetic to the existing landscape character whilst providing an appealing and quality construction.

3.0 LAYOUT

The evolution of the site layout of the proposed development has been evaluated in the earlier *Design* section. This section aims to describe how the layout works in more detail.

The new-build dwellings have been arranged on the site with consideration for the following:

Vistas and permeability

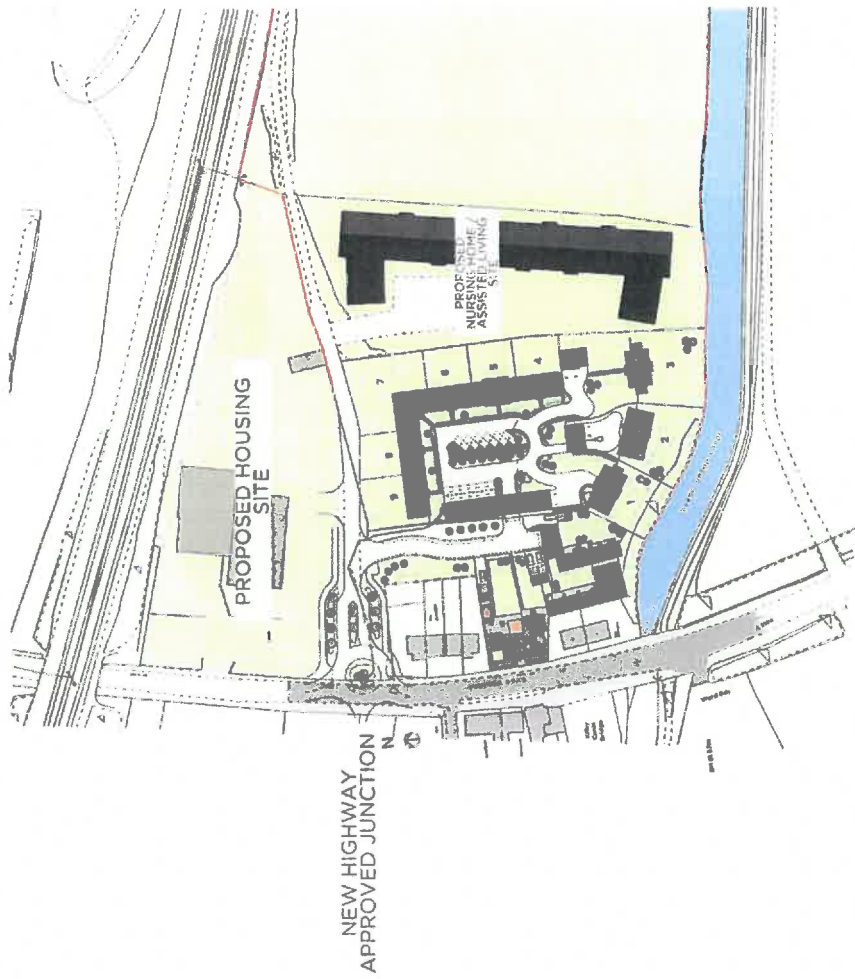
The site context is rich with landscape features that would add to the quality of new development. The site topography allows a natural vista towards the canal and River Soar beyond, as well as the open countryside / farmland. As indicated on the site layout below, the design seeks to frame and utilise these landscape features by creating visual permeability between and across the proposed dwellings.

Focal Points

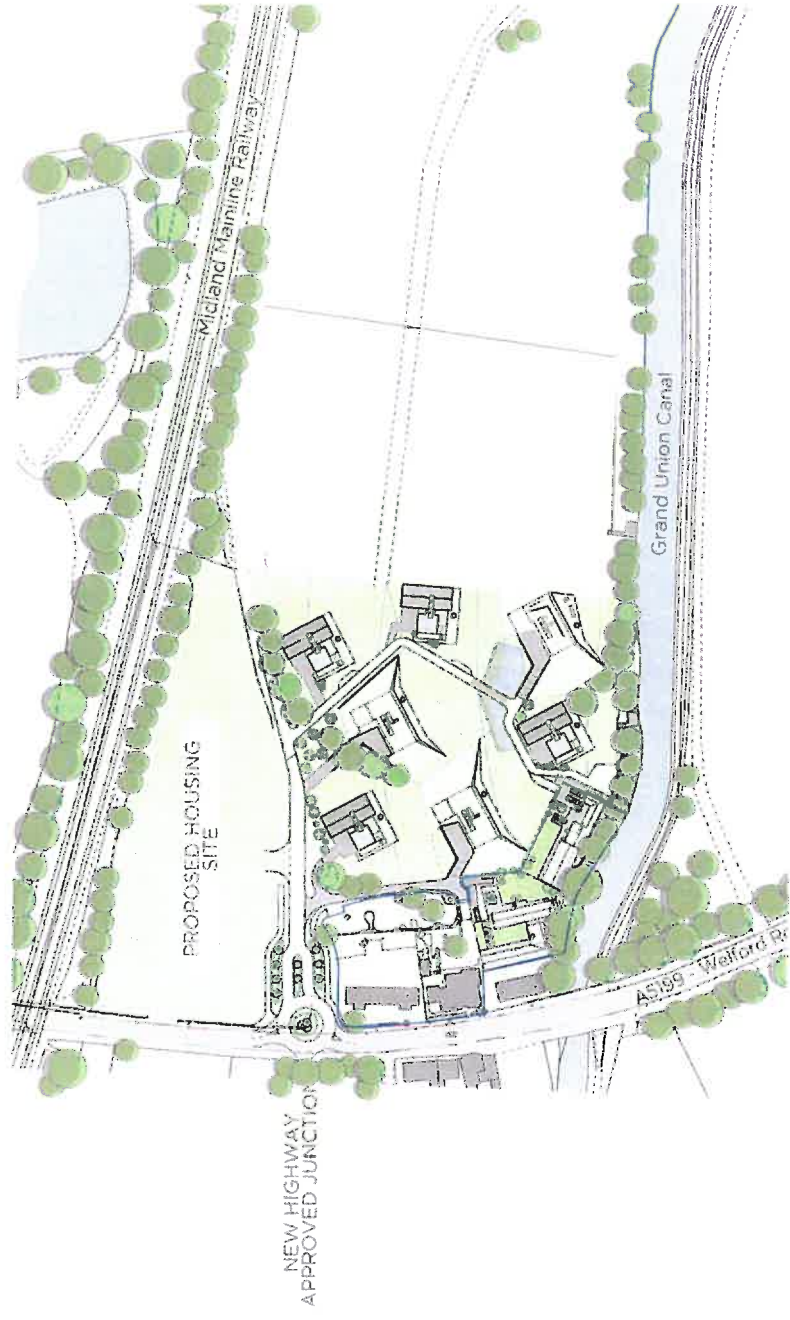
As discussed above, the site layout is seeking to create permeability. The inherent open character of the new development will therefore need to utilise focal points around the site for orientation purposes. Houses have been located around the site to offer focal points as visitors and residents enter and leave the development. In addition, existing trees have been used to identify key intersections in the infrastructure.

Orientation

As well as addressing the concept of vistas, permeability, and focal points, dwellings are orientated to best respond to sustainable design requirements. Houses are designed with large areas of glazing to the south elevation to encourage solar gain – reducing reliance on more environmentally costly heating devices. In addition, the houses are provided with areas of south / south-west orientated roof gardens. This orientation, together with the slope towards the sunlight, seeks to maximise growing potential for vegetables etc. Unlike photo-voltaic panels – solar thermal panels, for pre-heating hot water, can perform adequately without being orientated towards the sun as they rely on daylight.



Scheme for residential development and assisted living



Scheme for small scale residential development.

4.0 SCALE

The scale of the proposed development will alter existing views of the landscape. As such it is important to ensure that the effect of the changes on the people who benefit from the views is a positive one. Here we review the direct impacts of the development upon views of the landscape through intrusion or obstruction, the overall impact on visual amenity and the reaction of viewers who may be affected.

Scale & Character

The scale and character of the site and surroundings have been reviewed in earlier sections of this report; in brief however the area consists of a mix of development usage, although predominantly residential. With the site boundaries addressing the rear of a residential street scene, a large industrial scale site, open countryside, and the Grand Union Canal. The residential development, to the west of the site, is typically two-storey and of domestic proportions. The site itself is currently vacant, with small groups of trees encroaching from the northern, southern and eastern boundaries.

Visual Analysis

The new-build house types would be designed to provide an agricultural building aesthetic which is sympathetic to the site context. However, protecting the character of the existing settlement has been a key consideration and the development proposal seeks to ensure that the new buildings are not disproportionate – especially given the consistency in their intended uses.

5.0 LANDSCAPING

The proposal seeks to utilise and enhance the existing landscape, with both visual and sustainable agendas. The existing trees provide visual screening from the publically accessible towpath alongside the canal.

Plot boundaries are primarily defined by estate railings which offer minimal visual impact and are appropriate to the desired rural aesthetic.

Hard surface treatments will vary to suit the intended use – all of which are private and associated with individual plots. Gravel and permeable block paving will be provided to private driveways, minimising and attenuating surface water. Soft landscaping to private amenity spaces will be comprised of grass and shrub planting. Landscape has been a key consideration in the development of the design concept and sub-sequent development proposals. The proposals seek to utilise and enhance the existing landscape, with both visual and sustainable agendas.

Existing trees are used to define plot boundaries and routes around the site. In



Estate railings defining residential plot boundaries

conjunction with new tree planting, the existing trees provide visual and acoustic screening from features such as the mainline railway to the north. The emphasis on the development proposal's sustainability credentials can be seen in the site-wide proposals as well as the individual plot / house type designs.

6.0 ACCESS

Both vehicular and pedestrian routes to and from the development have been carefully considered in order to achieve the best possible solution to provide inclusive access for all.

Vehicular and Pedestrian Access;

The process in evaluating vehicular access has involved consultation with the Local Authority Highways Department in order to come up with a solution that provides good access to the site.

The highway solutions subject to approvals includes a new roundabout (6m radius to all entrance/exit points) to the (A5199) with separated "in and out" lanes into new housing development with feature landscaping. The scale of the carriageway reduces once into the site, where a spine road (5.5m carriageway width with 2m service strip to both sides) provides access to all areas of the development. shared-surface solutions reduce traffic speed and are appropriate to the small number of vehicles expected to be using the internal infrastructure.

As noted under section 7.0, shared-surface infrastructure will consist of reduced width carriageways interspersed with planting and street furniture and intended for combined vehicle and pedestrian use. The zone for the shared-surface route will include an allowance for a service strip and pedestrians, whilst vehicle passing places are provided to ensure ease of vehicular traffic flow.

As shown on the masterplan it is envisaged that the rear of the site and the existing SSSI will be extended and allow for the addition of public open space handed back to the local authority or held in a management trust as part of the new development opportunities.

Alternative Means of Transport;

Bus stops exist on Welford Road, enhancing the accessibility of the development for users of public transport.

Traffic Calming Measures;

Reduce speed rumble strips to be installed to existing A5199 carriageways on approach to new roundabout

Waste Storage and Collection;

Waste will be stored and collected from each plot. The road layout has sufficient space for collection vehicles to fully manoeuvre.

Summary;

The overall aim regarding access has been to enable inclusive access for all across the site area, via the easiest and most simple manner. This is achieved in all modes by incorporating clear and concise routes to, and through, the site.

Both vehicular and pedestrian routes to and from the development have been carefully considered in order to achieve the best possible solution to provide inclusive access for all.



Masterplan of the site allowing for the opening up of the area for public use, the creation of better links to the SSSI and canal sides.

7.0 SITE HISTORY

PLANNING APPLICATION INFORMATION FROM PREVIOUS SUBMISSIONS.-

A pre application submission was made in September 2016 and the response is outlined below. Addition to this the stables that currently exist on the site and the ménage have permission under 07/00235/FUL which is outside the current Kilby Bridge settlement boundary and outline planning for residential properties ref 13/00108/OUT which was withdrawn after consultation with the local authority due to the production of the new local plan, although this application was being viewed favourably.

Core strategy requirements were as follows.

Core Strategy Policy 1 Spatial Strategy for Development in the Borough of Oadby and Wigston seeks an approach to development that focuses development within the centres of Oadby, Wigston and South Wigston, then within the Leicester Principal Urban Area, and then within the Direction for Growth area. The new local plan brings Kilby Bridge into this focus.

Core Strategy Policy 3 Regeneration Schemes suggests that only appropriate development, such as small scale leisure, tourism and canal based development should occur within the settlement. Again the new local plan allows for residential growth in the area.

Core Strategy Policy 4 Sustainable Transport and Accessibility requires that in all new build schemes, the highways and transport infrastructure requirements needed to support that development must be considered and if development is of a significant scale a transport assessment and/or travel plan is required.

Core Strategy Policy 5 Green Infrastructure suggests that the Borough Council will seek a net gain in Green Infrastructure through the protection and enhancement of assets and the creation of new multifunctional areas of green space. It is worth noting that towards the far eastern area of the site lies part of a designated Site of Special Scientific Interest. The Grand Union Canal directly to the south is also part of the designated Site of Special Scientific Interest. Our scheme addressed this wish.

Core Strategy Policy 8 Climate Change and Renewable Energy, requires any application for new development to submit a ‘Sustainability Statement’, that demonstrates how carbon dioxide emissions will be reduced and by how much.

Core Strategy Policy 9 Flood Risk and the Water Environment requires that development should be safe from flooding and not increase risk of flooding elsewhere. In addition, all development over 1 ha in size must have a detailed Flood Risk Assessment carried out which identifies necessary mitigation and adaptation measures. A flood report has been completed for the site confirming it is not in a risk area.

Core Strategy Policy 14 Design and Construction; states that the Council requires high quality inclusive design for all new development and major refurbishment in the Borough.

Core Strategy Policy 15 Landscape and Character; in summary states that all development proposals will be considered against the need to protect and enhance the distinctive landscape and historic character of the Borough. The site is located within Countryside. The site also forms part of the Grand Union canal Conservation Area.

Highways Officer (Leicestershire County Council):

The department has concerns regarding the entrance's visibility splay and the sustainability of the location.

In relation to visibility, a new access point in this location would be contrary to IN5 Policy of the CHA current design guidance which states:

'Access to A- and B-class roads

1.29 We will normally apply restrictions on new accesses for vehicles and the increased use of existing accesses on:

- roads with a speed limit above 40 mph (that is 50mph, 60mph or 70mph) or where measured vehicle speeds are in excess of 40mph;*
- roads with a speed limit of 40mph or less which are essentially rural in nature.'*

The present proposal DAS / drawing 6193/202 refers to 45m of visibility being available. The department has recorded high speeds in the area, referred to below. Using the Manual for Streets stopping site distance calculator, the visibility splays that would be required are in excess of the 45m distance quoted by your client.

Speeds recorded approx. 40m to the north of the access in 2010:

- 39.5 mph in northerly direction = 65m visibility required;
- 37.8mph in southerly direction = 61m visibility required.

Speeds recorded approx. 120m to the south of the access in 2010:

- 37.8 mph in northerly direction = 61m visibility required;
- 43.1 mph in southerly direction = 73m visibility required.

Note: these speeds are historic figures, so if your client undertook an up-to-date speed survey and found that the speeds were different, then the access would need to be redesigned to the basis of these revised figures, and in accordance with the department's current design guidance with regards to widths radii, pedestrian visibility.

The department has told me that they would not generally encourage speed reducing features on main A-roads; however a scheme could be discussed with officers, to see if any appropriate solution would be possible.

Turning to the issue of sustainability and use of non-car travel, the DAS page 15 refers to the major development (reference: 13/00403/OUT) as including a school; however this facility has yet to be built. If the school is eventually constructed, and it would be close enough to your client's site and there are safe and suitable walking routes to and from it, then this may address the sustainability issues. If there is not a school in the required safe walking distance then the Authority are required to transport the children to school, which would add a financial burden to the Authority during times of lower Government funding.

I also consider that the LPA would have concerns about the narrowness and route of the access road as it approaches the highway and I would suggest that your client considers remediation measures to address these points.

Due to the development shown in the new local plan a new coordinated access arrangement would need to be designed for the site, without the land owned by my client the new highway arrangements would be difficult to achieve. Please refer to the master planned drawings with allow the old car showroom land to be developed in highways access terms. The whole of this site frontage is restricted due to the bridge. Access needs to be further to the south.

Ecology Officer (Leicestershire County Council):

Ecological surveys are required before commenting further.

The officer's advice does agree with Natural England (see below) but the officer would want to ensure any application is supported by up-to-date (within the last 2 years) ecological surveys covering a habitat survey and protected species such as badger and great crested newt. Surveys for otter, water vole and white-clawed crayfish would also be required if any works are proposed within 10m of the canal bank. The 10m buffer between the canal and the proposed development must comprise of a significant amount of semi-natural vegetation. It is not acceptable for this buffer to be made up of individual plot gardens. Any development around the pond would require ecological surveys including a habitat survey and protected species surveys for badger, great crested newts, otter, water vole and crayfish and your client is asked to note that any development in the SSSI, such as the fishing lodge, is likely to have a great ecological impact than elsewhere on the development site.

Environment Agency:

The Agency have no comments to make at this stage until they have receives confirmation that Natural England do not raise any objections to the proposal.

Natural England:

Further information required.

Natural England advises that your client contacts them for further guidance as his proposal may adversely affect the Kilby-Foxton Canal SSSI. The organisation did include some information based on an earlier proposal which I summarise for your convenience:

- any development must be able to demonstrate that foul and surface water should not be discharged into the Kilby-Foxton Canal SSSI;
- a construction management plan should be produced to explain the mitigation measures in place to avoid any damage to the SSSI;
- consider the impact on the biodiversity and geodiversity of the site, local landscape character and on local and national priority habitats and species;
- the proposed development is within an area that Natural England considers could benefit from enhanced green infrastructure provision;
- there is a 'reasonable likelihood' of protected species being present and so protected species survey and mitigation strategy may be required;
- provide opportunities to incorporate features into the design which are beneficial to wildlife, such as the incorporation of roosting opportunities for bats or the installation of bird nest boxes.

Canal and River Trust:

The Trust is not opposed to residential development in some form on the site.

The Kilby-Foxton Canal SSSI has been so designated because of its exceptionally diverse and well-balanced community of pondweeds. The banks and shallow margins of the canal support a range of emergent plant species which are home to invertebrates and bird species. The SSSI extends north from the canal to include the existing pond at the eastern end of the site. Care is needed to ensure that any changes to the profile of the existing canal bank and its environment does not affect the SSSI. The trust has concerns about whether the conversion of existing farm buildings to affect the canal bank as the structures are between 6-8m from the canal edge.

The canal is designated as a conservation area at this point, and the impact of the proposed development on the character and appearance of the conservation area will need to be considered. The proposed 10m landscape buffer should assist in screening the site to some extent, but careful consideration needs to be given to ensuring that there is no adverse visual impact on the conservation area.

The Trust owns a strip of land adjacent to the offside bank about 70m in length and extending to a depth of about 2-4m close to the buildings to be converted to form Plots 2, 3 and 4. Any access to, or oversailing of this land (or the canal itself) will require the prior consent of the Trust. No vegetation should be removed from our land without our prior consent.

Ellis Bridge (Br. 86), which crosses the canal by the western edge of the existing pond, is owned by the Trust. Any works to the bridge, or use of it, would require the Trust's prior consent.

Generally all the items shown on the above consultations could be addressed or have been superseded by the emerging local plan.

8.0 LOCAL PLAN

PRE SUBMISSION CONSULTATION DOCUMENT COMMENTS:-

Section 2.3.12 – Tourism plays a small yet important role in the area – this would and could be enhanced by opening up the lake and public open spaces as shown on the drawing for inclusion within an amended local plan. The change to a conservation area within private ownership would not be beneficial to the local community and tourism. A small car park would aid the traffic and parking issues in the area.

Section 2.4.3 – makes reference to the SSSI, with the new proposal as shown this area could be opened up to the public and extended into a new managed area. This would be made possible with the addition of a small scale residential extension as indicated within the report.

Section 2.5.1 refers to areas of flooding to Kilby Bridge, this site is not part of the flood zone this is to the south of the canal.

Section 2.6.1 refers to tourism, which would be enhanced by our proposals. Protection of the environmental and biodiversity would be enhanced by managed extensions to the SSSI and possible public access to areas currently in private ownership. Potential flooding issues, see above. The settlement is a gateway to the borough from the south, the current amenity is poor and lacks any coherent design, small scale development and enhancement to the existing buildings on the main road with new residential and possible nursing / assisted living properties would enhance the views into the area. The parking of cars on the verges and land in front of our site is currently detracting from the Gateway to the area.

Section 3.1.13 refers to increased opportunity to access open countryside to access open countryside in Kilby Bridge, this is currently not possible due to the lack of pathways and parking areas, our proposed scheme allow for the possible rebuilding of the bridge across the canal and the use of some of the private land for public access.

Section 3.2.1 objective 14 – refers to regeneration of the settlement of kilby bridge, but without additional areas capable of being developed this is hard to achieve, our small scale development allows for the enhancement of the area and better access enhancing the environmental and heritage access.

Section 4.2.11 – refers to the possibility of upto 40 new homes within the Kilby Bridge area, the whole of Kilby Bridge is around 20,000 sqm the useable, easily developed areas behind the housing to the west and the car showroom equate to around a useable area of 13,700 sqm which would give a very high development footprint which would create mass housing with small plots, totally against all other planning policies for the area. With the addition of an expansion of the Kilby bridge settlement boundary of around 13,000 sqm this would allow more sympathetic development of the area as a whole allowing the area to achieve the required number of houses. It must be remembered that the area of the existing settlement boundary includes some areas of flood zone 3, the extension of the boundary onto the ellis farm site is outside this flood area.

Section 7.1.1 refers to the redevelopment of the area as outlined above , 40 houses, tourism etc but without bringing in additional land for the housing and opening up current privately held land for the use of the public this can not be achieved.

Section 7.1.2 refers to future master planning of the area, this has already been shown as a possible masterplan on our scheme, without the addition of extra housing land the requirements of the local plan can not be achieved.

Section 7.1.4 – refers to the creation of a marina within the area, again reducing the number of housing sites to the area, a marina would not be financially viable without some development opportunities.

Section 7.1.5 – policy 17 refers to a minimum development density of 30 dwellings per hectare with is against the nature of the site as a whole and would lead to an overdevelopment density of the existing areas available for development. The policy goes onto state that the key objectives are to maintain the open, attractive and rural setting of the area, at 30 per hectare as a minimum this would not occur unless additional land is made available.

Section 7.4.13 – our proposal will actively encourage the promotion of healthy lifestyle etc by opening up a privately held land area for the public with additional walkways and footpaths.

The local plan refers to a wish list for the Kilby Bridge Settlement area which includes tourism, housing, marina, starter units and enhancement of the area considered as a gateway to Wigston. This has been addressed in our documentation.

9.0 CONCLUSION

We have been in consultation with the planning policy department and were led by their comments to include and promote the whole of the Ellis Farm site for a large housing scheme, we originally showed the smaller 6-7 property scheme outlined in this report. The planning policy team wanted to see the possibility of fully developing the site with sympathy given to the canal and the SSSI. See the scheme put forward for original consultation with the policy team for consideration as part of the emerging local plan.

This scheme has in effect been ignored.



Notwithstanding the above, the overall and underlying aim for this proposed extension to the development area has been to provide dwellings, community facilities, and a site layout that respond appropriately and sympathetically to the landscape and context with controlled visual impact and with sustainable design at its core.

The site proposed is well located for inclusion within the Oadby & Wigston master concept plan utilising an isolated area of land between the railway line and the Grand Union Canal. The proposals are sympathetic to the ecological and rural nature of the site whilst addressing the known housing need.

The environment agency have confirmed that the site lies outside of any known fluvial or tidal floodplains and is therefore at little risk of flooding.

The external alterations to the existing buildings serve to enhance the quality of the local built environment.

The proposed new-build dwelling concept serves to harness the vistas towards the surrounding open countryside and landscape features through careful orientation and arrangement on the site. This also, with the additional land allows for the development and opening up of the private land into public ownership.

The new-build dwellings and assisted living will seek to exploit the latest energy efficient technologies, whilst utilising site orientation and topography to ensure high levels of self-sufficiency and sustainability within the development.

The development seeks to minimise its ecological impact via implementation of landscape buffer zones, protection of habitats where required and the visual promotion 'Kilby-Foxton Canal' and 'Lake' Site of Special Scientific Interest. The SSSI would be extended and enhanced by the small scale development.

Each vista towards the site has been considered, ensuring that all views towards the site have been addressed; with subtle architectural interventions key to the development unveiling itself without imposing on the context.

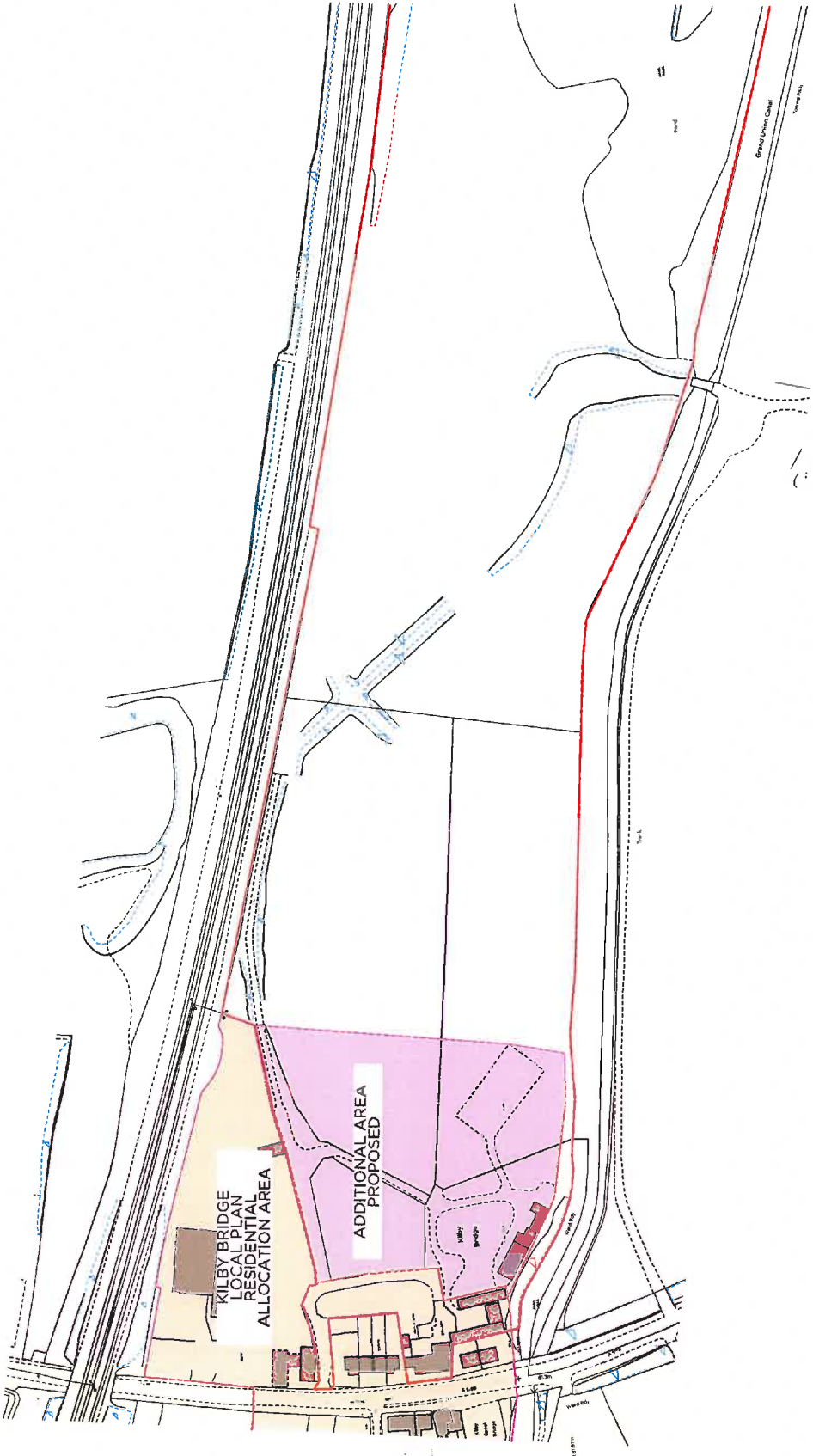
Pedestrian and vehicle links are integrated into the overall design and layouts, providing clear and simple access around the site with minimal impact on the surroundings. Enhancing and addressing an issue of parking on the main road.

The development sits at ease on the site both visually and functionally; working with existing site factors whilst providing elements of contemporary design that makes a positive mark on the landscape.

The proposed additional areas and opening up of the area as shown on the plans would meet all of the requirements outlined in the new local plan and aid in the aspirations outlined in the document, which we consider can not easily be achieved by the local plan in its existing form.

NOTES

- 1) THIS DRAWING MUST NOT BE SCALED.
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Revisions:		<input type="checkbox"/> APPROVAL	<input type="checkbox"/> INFORMATION
		<input type="checkbox"/> PRELIMINARY	<input type="checkbox"/> FOR CONSTRUCTION
Project:			
ELLIS FARM KILBY BRIDGE WIGSTON LEICESTER			
Client:			
MR R. AGGARWAL			
Drawing Title:			
EXISTING LAYOUT PLAN			
Drawing By:		Checked By:	
PJT		WB	
Date:		Scale:	
DECEMBER 2017		1:1000@A1	
Drawing Number:		Revision:	
888 / 800			

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Revisions:

APPROVAL	INFORMATION
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

Project
 ELLIS FARM
 MILBY BRIDGE
 WIGSTON
 LEICESTER

Client
 MR R. AGGARWAL

Drawing Title
 PROPOSED SCHEME 1

Drawing By
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Checked By
 WB

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Revision

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EXTENDED AREA FOR SSSI LANDSCAPED ACCESS ACCORDINGLY LAKE ENHANCED AND LIMITED

PUBLIC ACCESSIBLE PARKLAND WITH SMALL PARKING AREA

PROPOSED NURSING / ASSISTED LIVING LAND

PROPOSED ADDITIONAL RESIDENTIAL LAND

EXISTING ALLOCATED RESIDENTIAL LAND

NEW HIGHWAY APPROVED JUNCTION

REBUILDING OF BRIDGE CROSSING TO AID LINKAGES

Revisions:	
<input type="checkbox"/> APPROVAL	<input type="checkbox"/> INFORMATION
<input type="checkbox"/> PRELIMINARY	<input type="checkbox"/> RDR CONSTRUCTION
Project ELLIS FARM NILBY BRIDGE WIGSTON LEICESTER	
Client MR R. AGGARWAL	
Drawing Title MASTERPLAN LAYOUT	
Drawing By PJT	Checked By WB
Date DECEMBER 2017	Scale 1:1000@A1
Drawing Number 6103 / 503	Revision
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